

JAGUAR ASSOCIATION OF NEW ENGLAND

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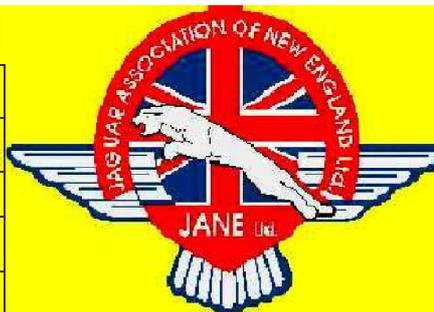
VOLUME 5, NUMBER 6

JUNE 2003



Clockwise from upper left - Chefs Chuck and Jack at MOT, Jaguars on the lawn at MOT, JANE members meet the new XJ8 at Woburn Foreign Motors, John Darack starting the slalom

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And blessed with might.

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To respond.
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but I am undoubtedly of the next century.

I am



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Jaguar Association of New England 2003 Calendar of Events

Events for the year which JANE will host, co-host or participate in as a club. Contact Wes Keyes 207-363-5338 or Dave Higginbottom 978-683-6278 for details

JUNE

JCSNE Concours, Berlin, CT.....June 8th
Track days, Watkins Glen, NY.....June 6-8th
JANE White Mountain Tour.....June 14-15th
Monthly Meeting, Skip's, Chelmsford, MA.....June 25th
British Car Day, MOT, Brookline, MA.....June 30th

Have an idea for an event ?

Give Wes a call

J A N E OFFICERS

President:	Carl Hanson	(781 275 2707)
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JANE on the Web

<http://www.j-a-n-e.org>

Regalia on the web, too! Order on line

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President's Notes – June 2003

By Carl Hanson

Well, as they used to say, June is busting out all over. So far the gloomy spring has shown little cooperation with our desire to drive our collector cars. We certainly could use a break. And we have planned a perfect event to take advantage of the usually good weather we have in June. The event is the JANE weekend tour of Mt. Washington Valley on the 14th & 15th, including a visit to the notable Moir Automobile Collection, beautiful Kancamagus Highway,, overnight at an upcountry hotel, and an exciting drive up the mountain road to the top of Mt. Washington



What a great photo opportunity your Jaguar on top of the world, but only if your modern Jaguar has a "1" on the shifter. Manual transmissions have no such restrictions and if your car is over 25 years old, you can go for free! See details in this issue, a previously sent email and postal mailer for information on this great event. Thanks to Pete Forte and

Wes Keyes for organizing the weekend.

Other events of note this month are the annual JSCNE Concours on Sunday, the 8th, at The Hawthorne Inn in Berlin, CT, the Historic Sports Car Races at Watkins Glen that same weekend – this year featuring Jaguar, and the Annual British Car Day at the Larz Anderson Automobile Museum in Brookline, MA, on the 29th.

The slalom program got off to a good start on Sunday, May 18th with a fine turnout of cars at a new site in Wilmington, MA. It helped to have a beautiful day, but a wide variety of cars showed up and we had a wonderful time. Well, some of us, had 'wonderful times'! Although there were a goodly number of Jaguars in play, we had to take our hats off to a race-prepared Triumph and a relatively stock Viper with the best times of the day. Never mind. We all had fun. Driving the JCNA slalom course reminds me of one of those car racing video games – only for real.

I would like to take note of the passing of Larry Shute, a loyal club member who regularly attended our meetings, parties and concours events. We missed him for awhile and learned that he had moved down to Pennsylvania to spend his final days near his son. We all have various memories of Larry. One of my recollections is that Larry was generally among the best-dressed attendees at our monthly meetings, wearing his signature blue blazer and ever-present tie. Godspeed, Larry. Wherever you are, I hope you are driving around in your favorite E-Type!

Have you checked out the Coventry Cat on our web-site? In beautiful color, no less! It really makes the 'Cat shine to see the cover, articles and ads in full color! If we could afford to print it that way, I am sure we would have won a prize or two in the JCNA Newsletter Competition!

That's it for June, folks! See you on the road.... Carl

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Restorations

By Carl Hanson

Veneering (Part III)

(Preface: This is the third and final article on re-veneering the wood on my XK120 FHC. If interested, you can find the previous two installments in the Coventry Cat section of JANE's terrific website – www.J-A-N-E.org.)

The previous two installments of this series told how I was able to re-veneer the wood for my Fixed Head Coupe. Now that the veneer has been attached with Pliobond #30, and it shows no sign of coming loose....oops! not quite. I should add some words about what happens when a bubble appears in the veneer. Yes, it happens even to the professionals. The solution depends on what kind of adhesive you used. Some, like Pliobond, are thermosetting --- which means that before the curing process gets too far advanced, it is possible to heat up the surfaces and the glue with a hair dryer or a warm iron. The heat reactivates the glue and softens the veneer to make it more pliable so you can press them back into contact. Maintain the pressure until the material cools down and likely you have removed the bubble. For more serious problems, or other types of glue, consult a book on veneering (e.g. Mike Burton's "Veneering – A Foundation Course, "Sterling Publishing Co., New York, 2000).

Let me start over... now that the veneer has been attached and the adhesive has been allowed to cure, it is time to apply the finish. And 'time' is the key --- because you will need lots of it. If you think you can get by with a five minute paint job, you are mistaken. A beautiful furniture-like finish takes a lot of time and handwork.

I followed a method described by Jay Lamm in his useful book, "How to Restore British Sports Cars," Motorbooks International, Osceola, WI, 1992, p. 103-104. He describes how to apply many coats, starting out with thinned varnish and working up to full coats. Instead of varnish, I used Minwax Helmsman Spar Urethane, figuring that the woodwork is exposed to various weather environments ranging from hot sun to freezing cold. Spar Urethane is tough enough to handle all types of weather on boats, so what more could one ask for? I selected the 'glossy' version rather than 'satin', with the understanding that one can always cut the gloss with rubbing compound to remove the excessively glossy finish that looks out of place on a vintage automobile. With 'glossy' you can control the final look, whereas 'satin' may end up dull no matter what you do.

My routine was to start with half-n-half (one-half urethane to one-half thinner) and paint it on with a cheap disposable foam brush. The next day, there is no sign of the previous urethane – it soaked into the dry veneer. Another coat follows with no

sanding between. Then the third coat was quarter-thinned (three-fourths urethane to one fourth thinner). Now the material has started filling the pores in the burlled walnut veneer. Wait another day. Then a light scrubbing with 150 grade sandpaper, or what I prefer, the abrasive pads put out by 3M – a product that acts like sandpaper without scratching the surface or filling up with material. Another coat of quarter-thinned urethane followed. A day later (see what I mean about time?), I rubbed that coat down, this time using 150 paper on a sanding block to remove any high spots in the finish. At this point, you can usually judge if the finish has stopped soaking in and is filling up the pores, in which case you start going to full-strength coats of urethane. Waiting a day between coats and sanding down the surface each time results in a smooth surface. How many coats you apply is your judgment call – use enough to eliminate the grainy seams and pores. The last few coats I wet-sanded with, first, 400 grit and last, 600 grit wet-or-dry sandpaper.

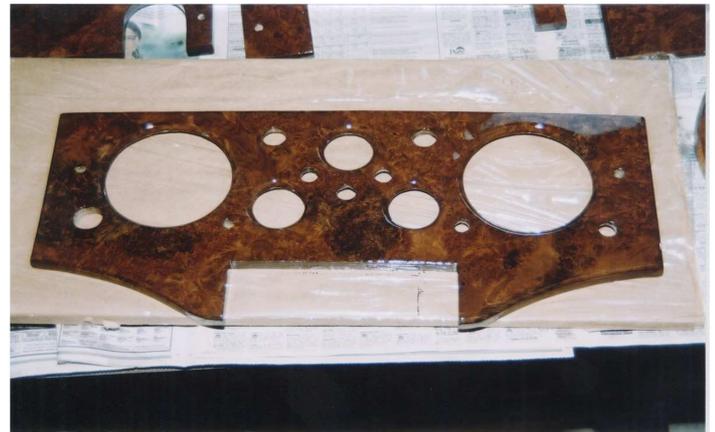


Figure 1. Completed Instrument Fascia

By this time the surface was glass-smooth. To make sure there were no brush marks at the end, I used the handy spray can version of the same type of spar urethane. This resulted in a smooth, glossy finish.

At this point you can decide how glossy you want the finish. I did not want a glossy finish on my veneer. In my opinion, glossy makes it look like plastic. So I made a rubbing compound out of linseed oil and pumice powder and rubbed that mixture all over the surface. I probably could have used ordinary rubbing compound purchased at the local auto parts store, but I followed the old school of furniture making. (*Oh by the way of a warning: if you ever use linseed oil on a cloth like this, do not crumple the cloth and throw it into the trash. It will ignite overnight by spontaneous combustion. I should know. I caused a fire in our house some years ago by absent-mindedly leaving linseed oil-soaked rags in a confined environment!*)

The final step is to wipe off the excess oil, wait another day or two, and apply a generous coating of carnauba wax. I used Butcher's Bowling Alley Wax. Rub it on, wait twenty minutes and polish the surface. The result is a deep, semi-gloss finish that speaks of well-cared-for antique furniture. Fabulous!

Figure 2. Right and Left Sides of the Fascia



The two pictures show the results of the project – the instrument panel and the right and left sides of the dashboard. Next step is to assemble the pieces, add the instruments and wait for the moment of installation.

Until next time!

Carl

A SPECIAL INVITATION TO JANE, OUR SISTER CLUB

Jaguar Club of Southern New England would like to invite all JANE members to attend our Concours d'Elegance Sunday June 8, 2003 and pre-Concours dinner Sat evening June 7th.

The show site is at the Hawthorne Inn in Berlin CT on the Berlin Turnpike and has excellent parking, space for trailers, overnight accommodations, pool, food concession and spacious grounds.

The Saturday night banquet is at Carmen Anthony's Fish House just up the road in Wethersfield. An excellent buffet is planned. Contact Virginia Raymond at 1-860-429-1003 for details and reservations.

Lodging reservations may be made at the Hawthorne, 1-860-828-4181. Be sure to mention the Jaguar Club

Concours information, call Ken Haas evening at 1-869-345-4260. If you are a judge and can help out, please contact Hal Kritzman days at 1-860-666-3045

JCSNE has supported JANE's Concours, please support your Southern New England friends.

Thanks, hope we'll see you there

Skip Barber Racing School – or my first day at the track, by Carl Hanson

I just got back from an exciting morning at the Skip Barber Racing School at Lime Rock Park. This was the "Introduction to Racing" school and it lived up to its name as an introduction, thereby whetting one's appetite for the full 3-day course where you actually learn how to drive these cars. It rained lightly, so the track was wet, making things a bit more interesting. Guaranteed spin outs. The class size was nine, none of whom had driven a race car before.

The first hour is a lecture on race car dynamics and tire characteristics given by Bob Green – who gave the lecture on historic race cars at the MOT earlier this year. After suiting up, we got a close-up description of the innards of the car used in the course and how they are operated. These light weight open wheel cars are 'Formula Dodges' pretty much the same chassis as was used in the old 'Formula Ford Series' but with Neon engines in them. They generate something like 150 HP.

The class then piled into a van for a trip around the course with an instructor showing us the lines to take --- special for this day due to the wet track. The usual dry-track lines are smooth, rubber filled and oily which make them extremely slippery in the rain. Finally we got into our assigned cars in groups of three following a Skip Barber Red Neon with instruction to follow his line exactly. We had three 20-minute driving sessions with short breaks lined up in the pits to get critiques and to rest our hands and arms. Afterwards, we had a group debriefing.

Our class had comparable driving skills, but I noticed the next class after ours had a great disparity in speeds among the drivers. To maintain order on the track, the Neons would pull into the pit area to reshuffle the drivers into groups with more compatible speeds. This was necessary because the groups of three would rotate driving behind the pace car on each lap, the change made on the main

Continued on page 9

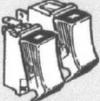


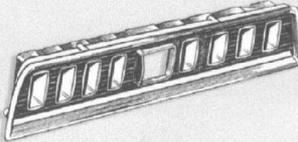
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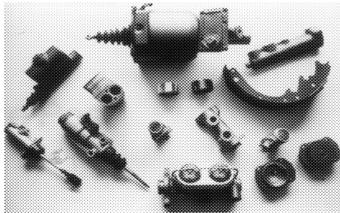
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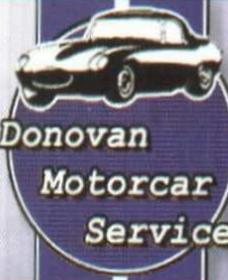
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SLALOM SEASON OPENS

MAY 18, 2003

The slalom season opened on a beautiful day at our new slalom site near the I93 and I95 intersection in Wilmington, MA, an industrial building complex parking lot.

The tech inspection and registration went off without a hitch. John Loring and Alan Quinn had laid out the course on Saturday and marked all the cone locations so while tech inspection was in process, the cones were spotted and the cars were ready for the timed runs.

Each car had at least four runs, all timed, and the best timed run was entered as the official run. The top three cars for the event was Bob Lang in his TR6 followed by John Loring in second in his Viper beating out Gary Hagopian by 0.1 seconds in third. Eric Hagopian probably had the best runs of the day, almost a second slower than dad Gary's Silver Hammer, but Eric made it around the course in nice smooth runs. The XK8 coupe seemed like it was on rails.



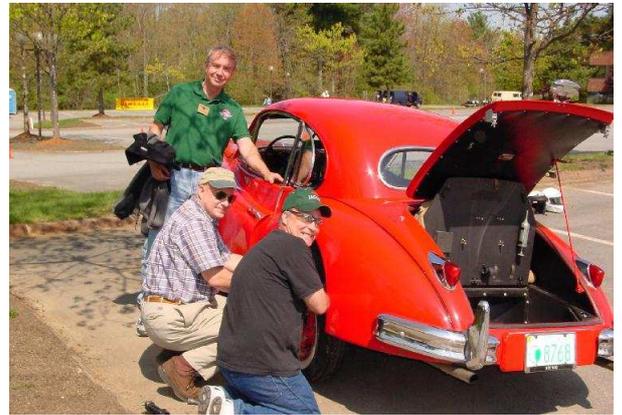
Bob Land in his TR6 had the fastest time of the day

The Hagopian family won both the attendance award and the number of runs award, Gary and Eric as mentioned, Doug with Felicia in Felicia's Z4 BMW roadster who let Doug's son Matt slalom her car (Matt's on a learner's permit) and Margo Hagopian Otey and her husband Brady and their Porsche coupe. Their sons Dean and Blake helped out with the cones while matriarch Sue Hagopian kept the scoring sheets. Matt Hagopian was very smooth in all his runs. I wonder what his Driver's Ed instructor would have said.

There were three early XK's and three late model XK's, a 120, a 140, a 150, XK8 coupe, XK8 convertible and a XKR convertible. All ran well, but Bill Starch has to get the hard luck award. He got a flat tire after one of his runs and he already had changed one before so with two flats he had to call for help. As he had driven down to the

event, his trailer with a tow car had to be dispatched from New Hampton, NH some 90 miles away to tow his car back home.

Bill Starch changing one of his tires



Chuck Centore on the course



The chutzpah award for the day was awarded to Alan Quinn who rounded the course in under 50 seconds in his Volvo wagon. Afterwards, wife Jessica took the wheel for a trial run while Alan rode shotgun. Baby Gabriel was content in his stroller watching Mom and Dad drive around the cones (baby sitter was provided ed.)

The smoke show of the day was won by Dean Cusano, JCSNE, in his XJR. He was supported by Ken Haas, JCSNE, with Ken's XJ6. Nice to have our sister club here at our event.

Quietest car of the day award went to Paul Rikert who ran his Honda Insight hybrid without tire smoke and squeal.

Lunch was served buffet style in between runs as John Loring, our Slalom chair, had ordered delicious sandwiches and soft drinks.

A great event on a beautiful day

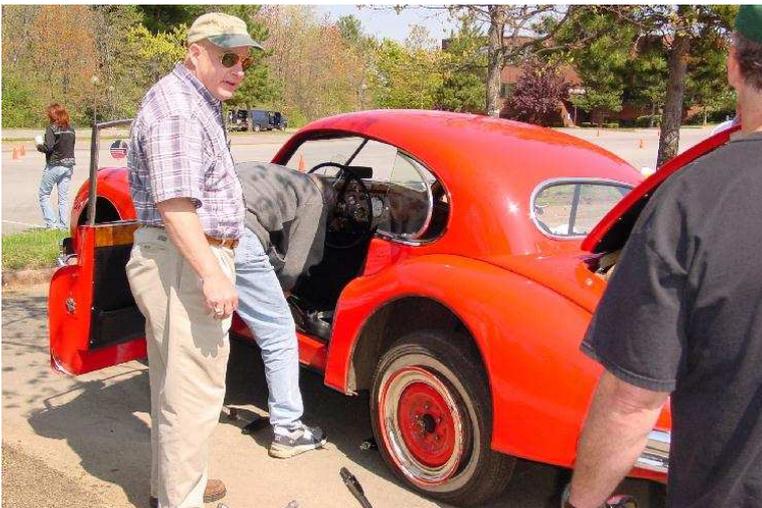
Pictures from JANE events



Race prepping Alan Quinn's Volvo for the slalom. Notice the feverous pace of the pit crew



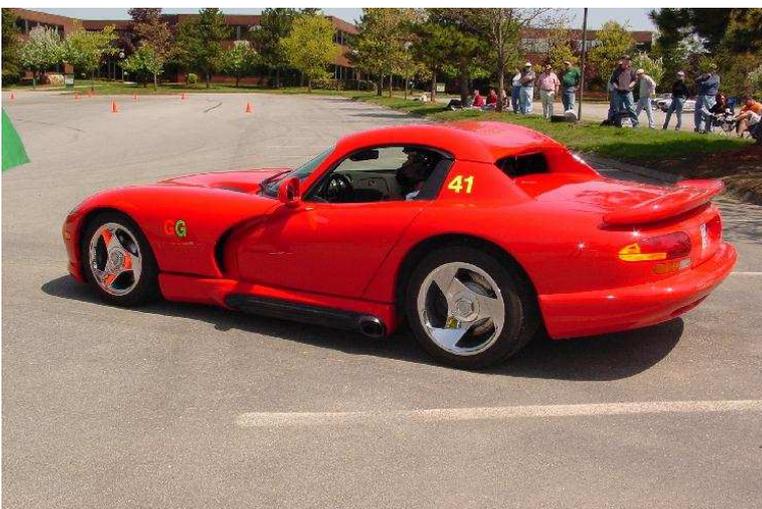
Carl Hanson/s TR6 at the slalom



**Above, Where's the jack? Anybody seen the jack?
Below, John Loring starts his Viper on a slalom**



Jim Roberge and his E type at the slalom



John Rizzi in his XKR at the slalom

Activities Calendar

Date	Event	Location
June 7-8, 2003	JCSNE/JCNA Concours	Berlin, CT
June 6-8, 2003	Historic Sports Cars Racing	Watkins Glen, NY
June 14-15, 2003	Overnight trip to the White Mountains	Ossipee, NH
June 25, 2003	JANE BOD	Skip's, Chelmsford, MA
June 29, 2003	British Car Day	MOT, Brookline, MA
July 12, 2003	Carburetor Tech Session	Sunapee, NH
August 8-10, 2003	JANE/JCNA Concours	Sturbridge, MA
September 14, 2003	JANE Slalom @ Site	Wilmington, MA
September 18-21, 2003	British Invasion, JCNA Concours	Stow, VT
September 29, 2003	Castle Hill Concours	Cancelled
October 4-5, 2003	Maine Museum Tour, Coastal Tour	
4 th Wednesday, each month	JANE BOD meeting	Skip's, Chelmsford, MA
2 nd Tuesday, each month	JCSNE meeting	Olympia Diner, Berlin CT

Wes Keyes, Activities Chairman

westonkeyes@hotmail.com

207-363-5338

Combined JANE/JCSNE/JCNA events

JANE Tour to Mt. Washington Valley

A great trip is scheduled for Saturday & Sunday, June 14th & 15th. Plan on joining in, reserve early!

Saturday, 10:00 A.M.: We'll meet in West Ossipee, NH at MacDonald's, the intersection of Rte. 16 & Rte. 25.

Our first stop: Chocorua; John & Heather Moir's car collection of "the alphabet, A to Z" and then some! The collection consists of some fifty autos dating from 1906, mostly British.

Next stop: Lunch at the Brass Heart Inn. Dating back to the 1800's, the inn has all the charm you could ask for, and an excellent reputation for fine food.

Next the Drive: We'll cruise the Kancamagus Highway, with a stop at a scenic vista; through Lincoln; then by the Mt. Washington Hotel. (Possibly stop in at the hotel.)

Finish: Bartlett; At our lodgings, the Grand Summit Hotel for check-in. Call in your reservations to 888/554-1900 Mention the "JANE Tour" when calling in.

Sunday, 9:00 A.M.: To the Auto Road: It's off to the Auto Road for a drive to the top of Mt. Washington.

Next: A BBQ/Lunch at the base of the Auto Road. Sure to fill any empty spots! (We'll need a head count for this lunch too.)

After Lunch: On your own; Visit North Conway discount stores or a leisurely, and safe drive home...

Racing School (Continued from page4)

straight where top speeds were somewhere between 80 and 100 mph.

If one driver was too slow, the others would have to practically stop to let him/her move up to the pace car.

Most of us spun out at least once -- I spun out onto the grass at turn 2 when the guy ahead of me spun out. In retrospect I must have let off the accelerator too quick at the end of the turn -- the equivalent of braking the rear wheels -- that was guaranteed to make me spin. One of the more aggressive drivers spun out three times. Driving in the wet taught us some important lessons, but I would rather have learned how to handle the car under better conditions.

I will have to admit I have gained a great deal of respect for the race drivers who brake, shift, pass cars, dodge spinning cars, and still live to tell about it. Me? I was on sensory overload just trying to keep up with a Neon pace car! I believe the 3-day course would be better for making one feel more comfortable on the track.

In short, I enjoyed it, but wish it could have lasted a bit longer -- on a nicer day. No fault of Skip Barber, of course. After all, it was an introduction!

A highlight was when David McHugh -- a VP at the track -- came by to introduce himself to me because he knew of my connection to JANE! He is a fan of the early Jaguar sports cars, with experience on XK120's, 140's and E-Types. Anybody want to make this a JANE event sometime?

Guest Speakers at MOT, 13 May 2003

Jonathan A. Stein

New Jersey native Jonathan A. Stein is a graduate of both the Princeton Day School and Drew University. An automobile hobbyist for the past 28 years, Stein is best known for his work at *Automobile Quarterly* from 1988 to 2000 and for numerous freelance magazine articles in both American and British publications. Stein now edits automotive books for Bentley Publishers, contributes to several automobile magazines, and consults for automotive auction houses and museums. Stein wrote an article on JOMAR in the *Automobile Quarterly* and has followed the marque for years.

JOMAR

Just 24 Jomars were built between 1955 and 1959. All but two of them were TVR-based. At first, just the completed chassis with engines and running gear were dispatched from Blackpool, England to the Merrimack Street Garage in Manchester, New Hampshire. These early cars were powered by Coventry Climax engines, paneled in aluminum and used

exclusively as sports racing cars. Soon, however, complete cars were assembled in England and fitted with fiberglass bodies. Engine choices included side-valve Ford with or without a supercharger, or the Coventry Climax unit. Although a single roadster was built, the balance were either notchback or fastback coupes.

Early TVR Granturas and Jomars are virtually identical, but only the cars sold through Ray Saidel's Merrimack Street Garage wore the round JOMAR badge. Very few Jomars may have been built, but the Merrimack Street Garage, once New England's largest imported car dealership still thrives selling Volvos and Oldsmobiles. And Ray and son Alex Saidel have kept the obscure marque very much alive through their vintage racing.



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For Sale: 1987 Series III XJ6, pristine, original condition, only ,53K miles. No winters. Garaged. Nardi steering wheel, A/C conversion to R134 refrig. Dorchester gray, red interior, books and manuals, \$8700.00 Call member Bill Read ay 608-945-6022 or email at billbany@msn.com 0403

For Sale: 1958 Austin Healey Bugeye Sprite 3/4 through a concours restoration of a very, very early Bugeye Sprite(April 1958); Over \$10,000 spent on the body shell alone. Will sell outright or trade for a XK120-140-150 project or a XJ6 series 1-3 in solid shape. Other Jaguar trades considered. What do you have? Email for more including pictures dthompson@gbc.com or phone 514-848-0716 0403

For Sale: 1994 Jaguar XJS V-12 Convertible; Black/Tan, 57K Original Miles, Loaded; Respectfully Maintained - All Papers & Manuals; \$20,500 OBOF - 508-457-6262 (Cape Cod) 0403

For Sale: 1992 Jaguar XJ6 Sovereign. Palomino/Tan. 127K well cared for miles. Excellent condition. Sunroof. Michelin XV's on summer wheels. Pirelli winter/ice on winter wheels. Needs a good home. \$6,900.00 OBO. Call Kevin Murphy, (508) 820-7759 days (508) 879-1881 eves. Or email kevin@kmurphyslaw.com 0403

For Sale: 1964 XKE Series I FHC. Low miles, owned by Jaguar mechanic - all mechanics rebuilt; new interior, very nice; white wire wheels; many new parts; Call JANE member Dr. Glick, 603-539-5830 or email marbfram@localnet.net 0603

For Sale: Two XK150 air cleaner assemblies. One unrestored, nearly complete, \$75. The second has been professionally restored and is beautiful, \$300. Both include aluminum plenum. **Five painted (red) wire wheels**, four with Avon tires mounted, one with a nearly new Firestone. Fit XK120, 140,150, very good condition, \$350 for all. John Darack, 508-653-5094, DPisland@earthlink.net 0603

For Sale: Jaguar XK120,140, & 150 new sheetmetal: front and rear wings (patch panels or complete), spats, rear inner wings, fuel tanks, doors, door skins, boot lids, boot lid skins, sills, shut pillars, battery boxes, stainless steel side curtain frames and more! Rebuilding service for your doors, lids and door hinges. All parts fabricated by WES PARTS - over 30 years experience. Call or write Wray Schelin, WES PARTS, 508 347 7749, PO Box 652, Charlton City, MA 01508. For a free catalog. E-mail: wesparts@hey.net pd

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Please contact David Roth at the address below or email him at RothHollis@aol.com to get a quotation.Please send camera ready copy and a check before the 15th of the month previous to intended insertion. Make checks payable to JANE, Ltd.

Mail to:David Roth,180 Wheeler Road, Hollis, NH 03049-5930

MOT Lawn Event was usual success

The annual MOT lawn event had the usual fine turn out even though the weather looked threatening.

Master of the barbeque Jack Capocefalo set up the grilles ready to go at 5:30 and had the usual fine fare of dogs, burgers, and sausage. The package deal fixed menu at \$5.00 proved popular with several choices available. Chuck Centore was prepared with his Italian chef's apron and assisted Jack at the grille. Nancy Capocefalo and Patt Centore took care of the food tickets and cash while the chefs did their thing on the grilles. This year the "Italian kitchen" was as smooth as could be compared to last year's which was more like a 1950's Ferrari pit stop.

Some fifteen Jaguars lined up on the lawn for some "show and tell". Several members still didn't have their cars on the road yet, not surprising considering the winter and the spring we've had in the Northeast USA. Bruce Murray, a new JANE member, showed his nice E type FHC driver to club members for the first time. Wes Keyes no showed for the lawn lineup....rumors were that he had some trouble on the road with his E type coupe. Adrian Curtis upheld the XK marque with his XK120 DHC. All in all, very nice showing of cars by all.

The indoor event, a MOT lecture by Jonathan Stein, followed. He presented a historical prospective of JOMAR, New England's own marque. Alex Saidel son of the JOMAR founder, showed two of the cars as a part of the lecture.



Ask Chuck for the translation
if your Italian is a bit rusty



The "Italian Kitchen",
Jack Capocefalo and Chuck Centore

LIME ROCK PARK TRACK DAYS

For all of the JANE members with a need for speed, there's a perfect event scheduled for you this month. It's at Lime Rock Park at Lime Rock, Connecticut on the 30th and 31st of May.

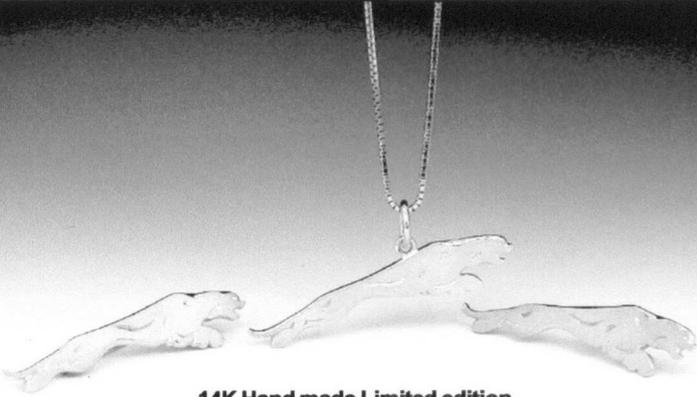
The first day is tech inspection of your car (minimal for safety requirements) followed by drivers instruction on the course, first with an instructor on board and then without. The day ends with a dinner in a tent with a live jazz band and a floor for dancing.

The second day, you take to the track for timed runs. You share the track with other cars, but there's a no passing rule to keep things safe and sane.

The event is sponsored by the Empire Division of JCNA and the Vintage Sports Car Club although this is not a JCNA sanctioned event.

There is camping, shower, and bath room facilities on site for hardy souls who like the great outdoors.

Contact Charles Bordin, 914-725-1274 (H) or 914-968-5700(W) or visit the JCNA web site at www.jcna.com for more information.



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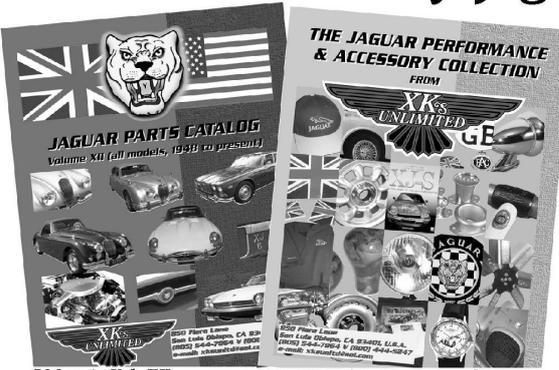
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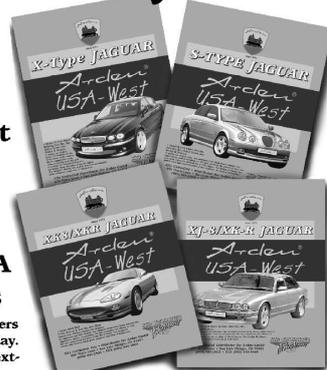


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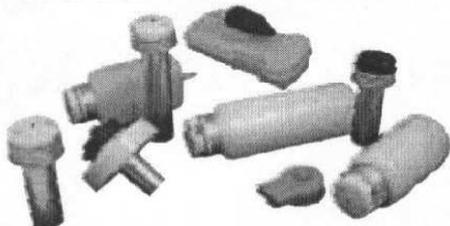
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